



Artist's impression of Sydney Olympic Park metro station.

Major work underway at the Sydney Olympic Park metro station site

May 2022

The NSW Government is delivering Sydney Metro West, a new underground metro railway which will double rail capacity between Parramatta and the Sydney CBD, with a target travel time of about 20 minutes between the two centres and 15 minutes between Sydney Olympic Park and Sydney CBD.

Sydney Metro West stations have been confirmed at Westmead, Parramatta, Sydney Olympic Park, North Strathfield, Burwood North, Five Dock, The Bays, Pyrmont and Hunter Street in the Sydney CBD.

Sydney Metro was granted planning approval to construct twin underground rail tunnels between Westmead and The Bays for Sydney Metro West in March 2021.

Acciona Ferrovial Joint Venture (AFJV) has been awarded the contract to deliver 11 kilometres of twin metro rail tunnels between The Bays and Sydney Olympic Park and excavate five new metro stations.

Sydney Olympic Park construction site

Major work to establish the Sydney Olympic Park metro station construction site is now underway with demolition of Sydney Metro owned properties expected to continue until the end of May.

Following demolition of the buildings, ground levelling work will be carried out in preparation for site establishment activities which will continue over the next few months. This work will include minor civil works such as construction of internal access roads, utility connection work, construction of a piling platform as well as set up of site amenities such as temporary offices, a workshop, a water treatment plant and station box civil work.



Demolition progress as viewed from 7 Figtree Drive, Sydney Olympic Park.

Key activities

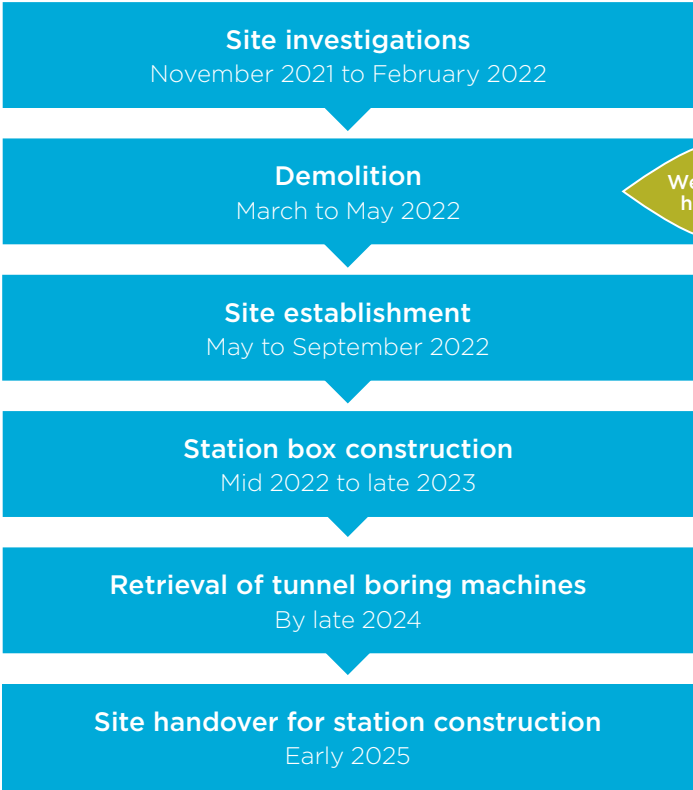
Demolition of existing Sydney Metro owned buildings at 5 Figtree Drive and 8 Herb Elliott Avenue and clearing of the site will continue until the end of May.

Site establishment including construction of all required structures, workshops, offices, facilities, power and water services.

Station box construction begins with the installation of interlinking concrete columns around the edges of the station box. This concrete retaining wall provides ground support during excavation of the station box that is around 25 metres deep, 37 metres wide and 198 metres long.

Tunnel boring machines are expected to start tunnelling from The Bays by the end of 2022 and reach the Sydney Olympic Park site by late 2024. At that point, the TBMs will be dismantled, lifted out of the shaft with cranes and loaded onto transport vehicles.

Site handover once tunnelling is complete, AFJV will hand over the site to the contractor responsible for delivering the station construction works.



Six month look ahead

Activity (subject to change)	May	Jun	Jul	Aug	Sep	Oct
Demolition	●					
Installation of site amenities	●	●				
Utility disconnections, relocations and installations	●	●	●			
Station box piling	●	●	●	●		
Capping beam installation			●	●	●	
Station box excavation					●	●
Anchoring					●	●

Construction activity ●

What is piling?

Piling is construction of underground columns that provide vertical and horizontal support to a structure. Before the Sydney Olympic Park metro station box excavation starts, a series of piled columns will be drilled into the ground to form the foundations for the station box.

This work will be carried out by piling rigs which drill a series of shafts into the ground before steel cages are installed and concrete is poured to form concrete columns. A series of these piles will be drilled along the outer edge of the station box.

Two piling rigs will be used at the Sydney Olympic Park metro station site and they are expected to arrive in late May with piling continuing until around September 2022.

Traffic and haulage routes

Heavy vehicles will enter the construction site via Olympic Boulevard and Herb Elliott Avenue and exit via Figtree Drive and Olympic Boulevard (please refer to map below). This haulage route was recently approved to improve traffic flow in the area. Alternate routes will be used during major events within the precinct.

Two on-street parking spaces on Herb Elliott Avenue directly in front of the site may need to be removed to create a safe entry and exit point for trucks during construction. Every effort is being made to keep changes to a minimum and more information will be provided over the coming months.



A piling rig at The Bays Station site.

Approved haulage routes



Out-of-hours work – six month look ahead

While tunnelling activities will occur 24 hours per day, seven days a week, other work activities are generally carried out during standard construction hours. Some activities such as utility investigation work or work on busy roads must be carried out at night for the safety of workers, motorists and pedestrians.

Please see below an overview of potential out-of-hours construction work for the next six months. Depending on the type of work and expected impacts, a range of mitigation measures may be used such as completing

the noisiest work before midnight, installing noise blankets around the worksite or providing alternative accommodation.

The project team will liaise with directly affected residents to provide more information and discuss mitigation options before the work. If you have an enquiry or would like to provide feedback about the work, including appropriate respite periods, please contact the AFJV community team on the details provided below.

Activity	Location	Noise impact	Timing
Environmental management and emergency work	Only required in case of emergency	Subject to nature of work	Only required in case of emergency
Oversize equipment deliveries	Station site	Low	As required

Second tunnelling contractor on board

Sydney Metro has awarded Gamuda Australia Laing O'Rourke Consortium (GLC) the contract to deliver nine kilometres of twin metro rail tunnels between Sydney Olympic Park and Westmead.

GLC will start early works around Sydney Olympic Park later in the year. GLC's tunnelling work will start at the Rosehill Services Facility and progress towards Sydney Olympic Park in late 2023.

AFJV and GLC will work together to manage any potential cumulative impacts during construction of the Sydney Olympic Park metro station.

Construction hours



Work at the Sydney Olympic Park metro station site is approved to be carried out during the following standard construction hours:

- **Monday to Friday 7am to 6pm and Saturday 8am to 6pm**
- Tunnelling and cross passage excavation below ground is approved to occur 24 hours a day.

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Contact us

-  **1800 612 173** Community information line open 24 hours
-  **MetrotunnelsAFJV@transport.nsw.gov.au**
-  **Sydney Metro West**, PO Box K659, Haymarket NSW 1240
-  If you need an interpreter, contact TIS National on **131 450** and ask them to call **1800 612 173**